

# Story Avenue Bus & Safety Improvements

Bronx Community Board 9 | April 1, 2021



# Background

# Better Buses

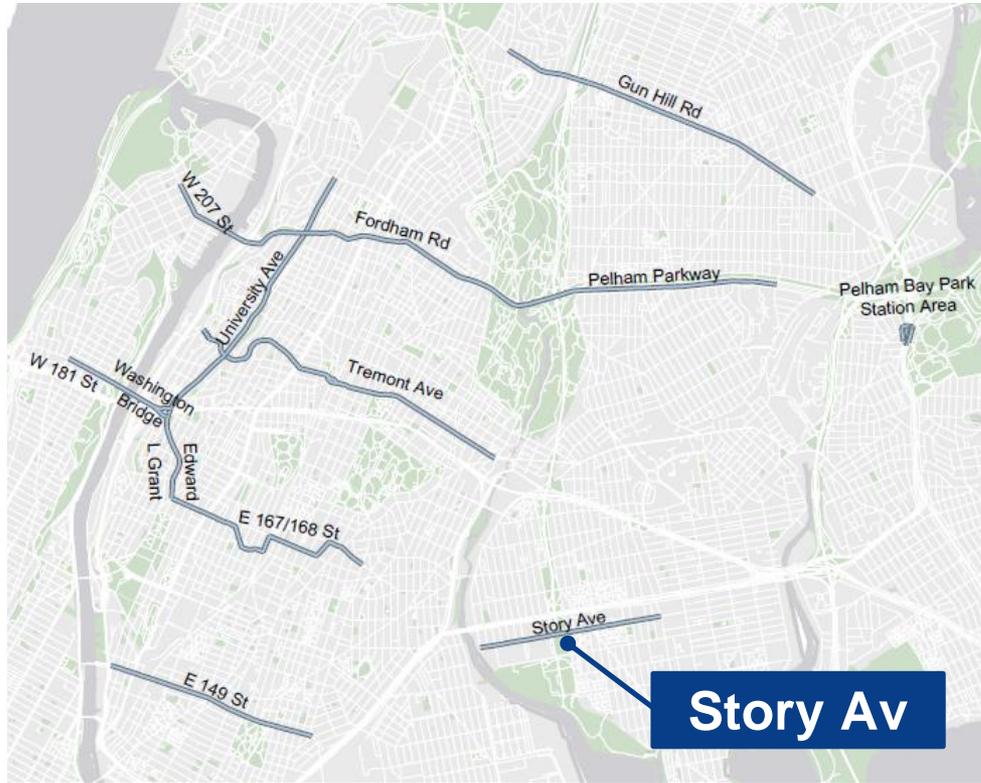
## Better Buses Program

- Mayor's 2019 State of the City:
  - Improve bus speeds 25%
- Better Buses Action Plan
  - Released April 2019
- Bus Priority Projects
  - 22 projects implemented in 2019
  - 16.3 miles of bus lanes installed 2020
  - 2 projects completed in the Bronx in 2020:
    - Edward L Grant Hwy
    - E 149 St



# Better Buses

## Bronx Bus Priority Corridors



- Analyzed major Bronx corridors to identify locations where treatments to improve bus speeds and reliability are needed
- Ranked potential bus-priority corridors using the following criteria:
  - Demand for bus service
  - Bus performance (speed and reliability)
  - Service levels proposed by MTA
  - Neighborhood demographics
  - Feasibility of implementation
- Selected 10 of the highest ranking corridors and have begun studying bus priority projects in the Bronx. NYC DOT work began in 2020.

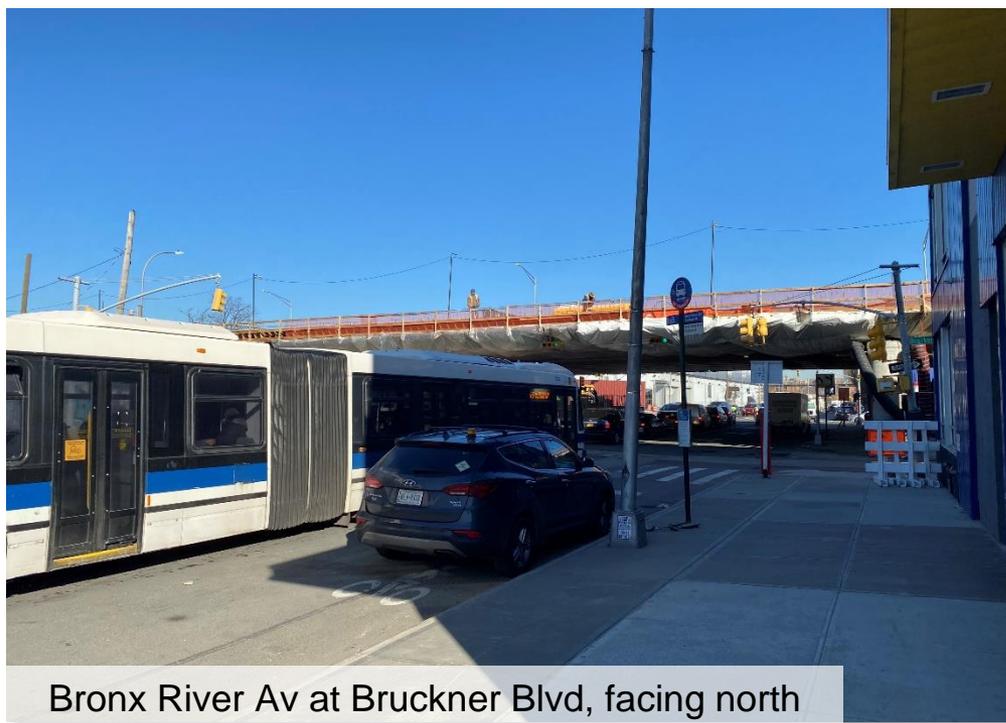
# MTA Bronx Bus Network Redesign: Story Av



- MTA Bronx Bus Network Redesign currently on hold
- MTA Bronx Bus Network Redesign proposed to:
  - Re-route Bx6 SBS east along Story Av
  - Create new Bx6 SBS terminal at Turnbull Av and Pugsley Av
  - Serve Hunts Point by Bx6 Local
  - Bx5 would continue to run on Story Av
- Proposed 2021 project will continue with some of the bus priority changes ahead of MTA's Bronx Bus Network Redesign and service changes
- Proposed 2021 project will improve existing critical bus service on Bx5, enhance east-west connections, and support future changes that would increase bus frequency on the corridor

# Reconstruction of Bruckner Expressway

- There is currently a NYS DOT project reconstructing more than a half-mile section of the Bruckner Expressway (Interstate 278)
- Construction at Bronx River Av and Bruckner Blvd may impact timeline of any changes at the intersection
- DOT will continue to monitor the area and will coordinate with State DOT and the MTA

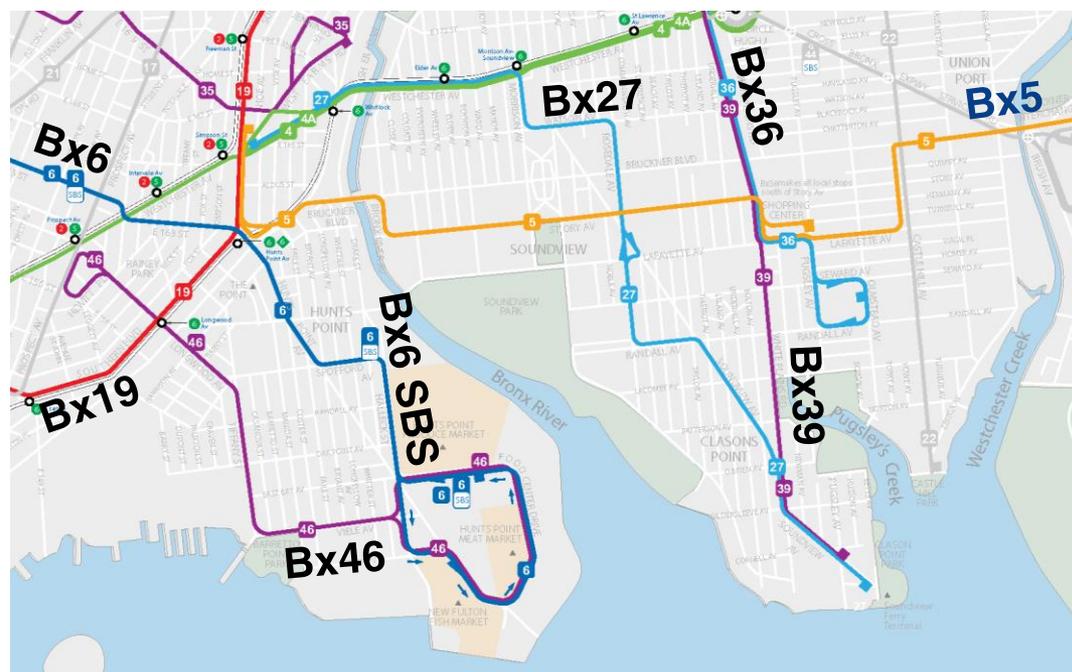


# Existing Conditions

# Story Av

## Overview

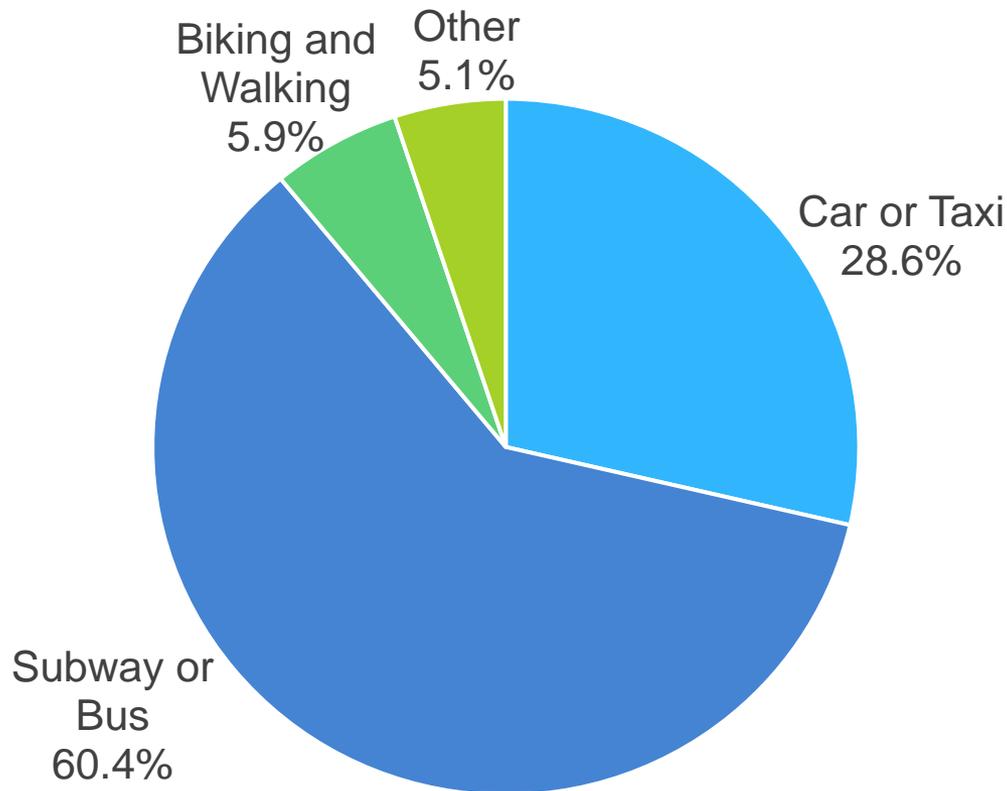
- Routes: Bx5
- Average weekday ridership:
  - Bx5: 14,781 daily riders (May 2019)
- Key connections to:
  - Bx6 SBS/Bx6, Bx19, Bx35 and several others
- Average bus speeds:
  - 5-6 mph during peak periods
- Key issues: Slow bus speeds, traffic congestion, double parking/loading, long distances between pedestrian crossing



# Community Overview

## Bronx Community Board 9

Primary Modes of Transportation to Work in CB9



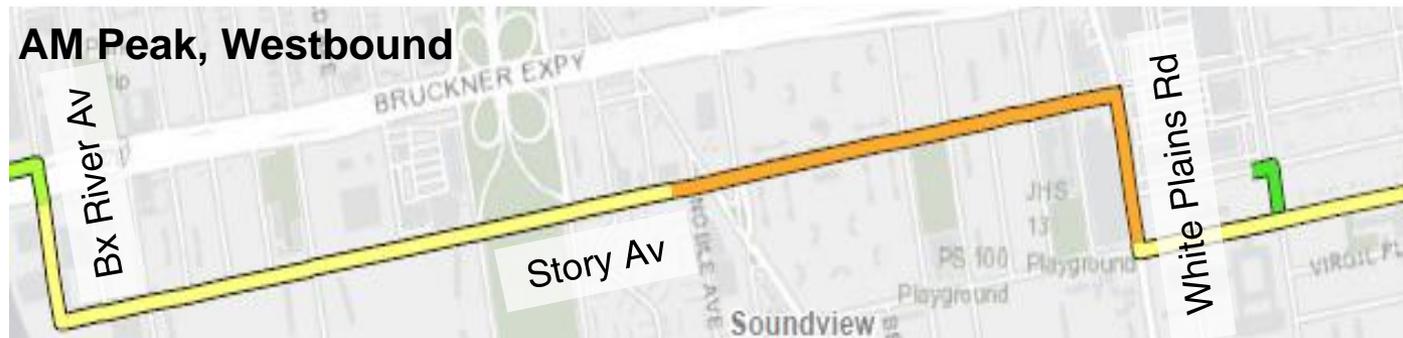
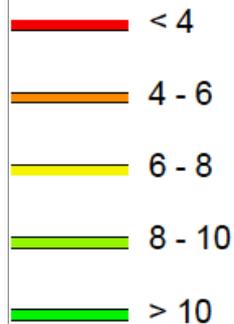
- Majority of residents living in Bronx CB 9 use public transportation to get to work
  - 60.4% use public transportation
  - 28.6% use a car/carpool
- Despite high ridership, minimal space dedicated to buses, resulting in slow bus speeds
- Bus and street improvements to the Story Av corridor would directly benefit CB 9 residents

Data source: US Census Bureau, American Community Survey 2019

# Existing Conditions

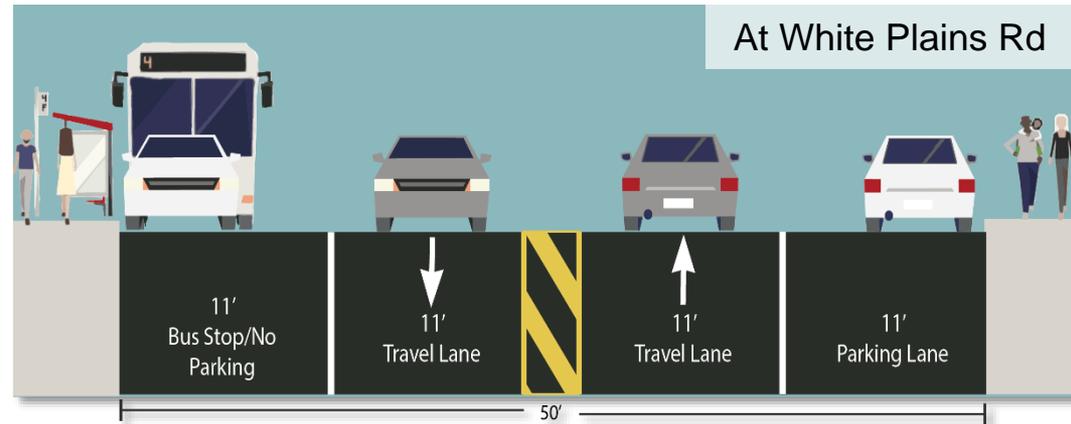
- Bus speeds are slowest during peak periods
- Speeds drop as low as 5 mph

## Bus Speeds (mph)



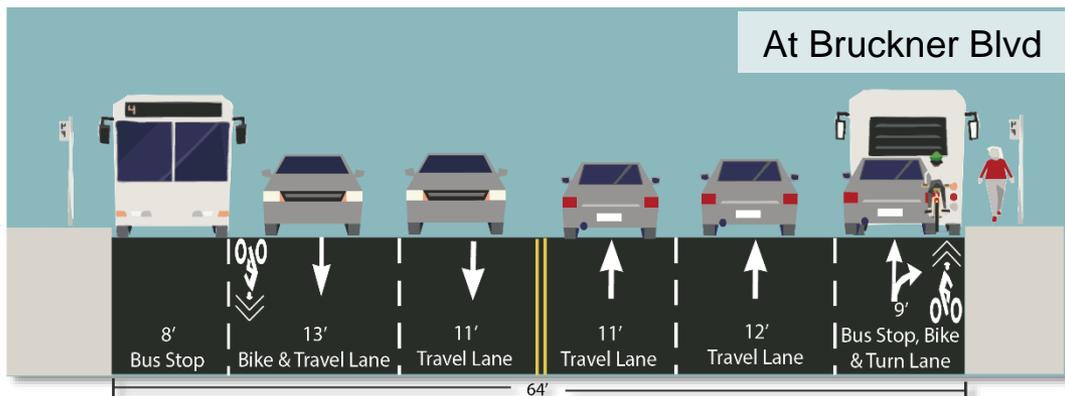
Data source: NYCT Bus Time Data, Bx5 Local Speed Data, Oct 2019

# Existing Conditions on Story Av

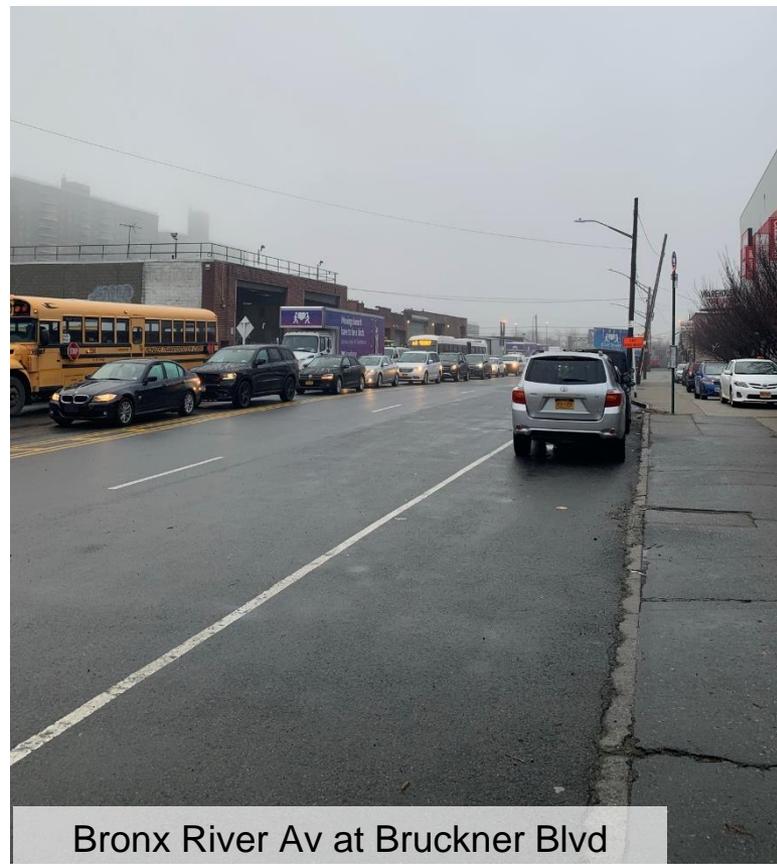


- Key Issues:
  - Peak hour bus speeds drop to 5 mph
  - Double parking, weaving & merging
  - Vehicles blocking bus stops
  - Traffic congestion

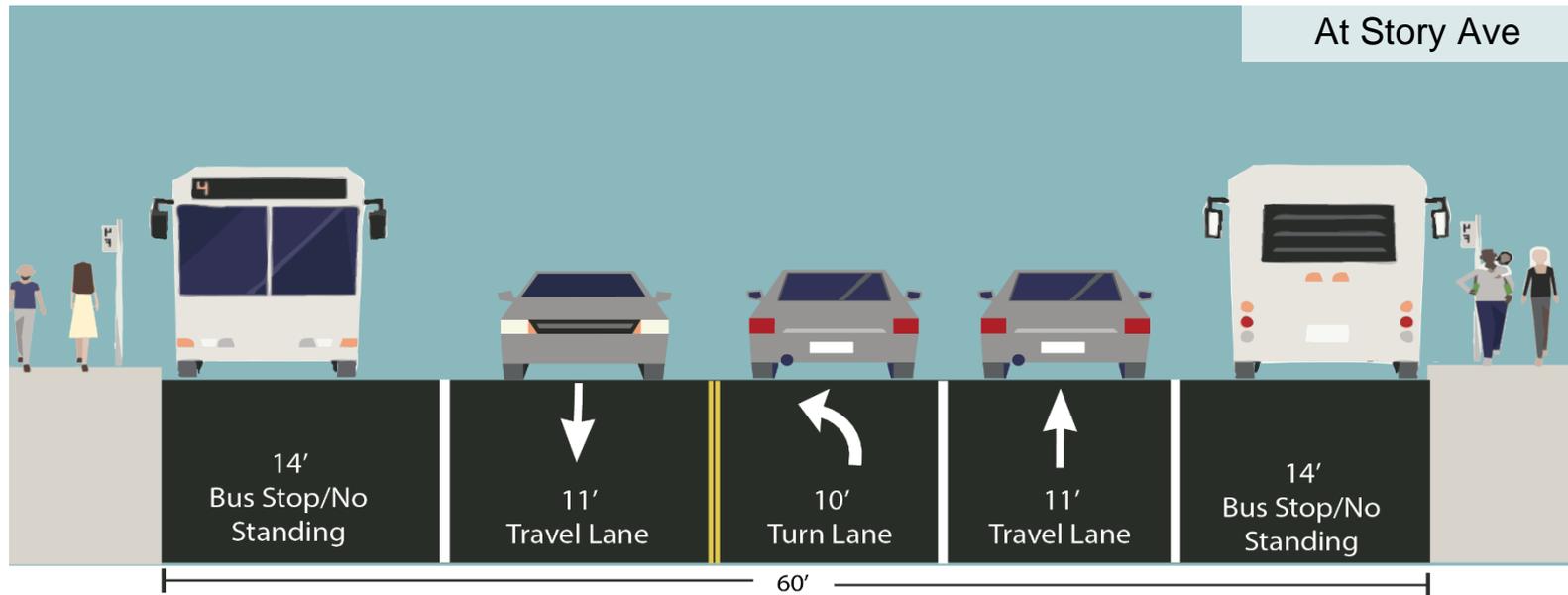
# Existing Conditions on Bronx River Av



- Key Issues:
  - Peak hour bus speeds drop to 5 mph
  - Double parking, weaving & merging
  - Vehicle blocking bus stops
  - Lanes often blocked, heavy truck and auto activity
  - Traffic congestion



# Existing Conditions on White Plains Rd



- Key Issues:
  - AM Peak hour bus speeds drop below 5 mph
  - Vehicles blocking bus stops
  - Parking and double parking in No Standing areas of curb in front of shopping plazas

# Traffic Safety

## Story Av, Bronx River Av to White Plains Rd, BX Injury Summary, 2014-2018 (5 years)

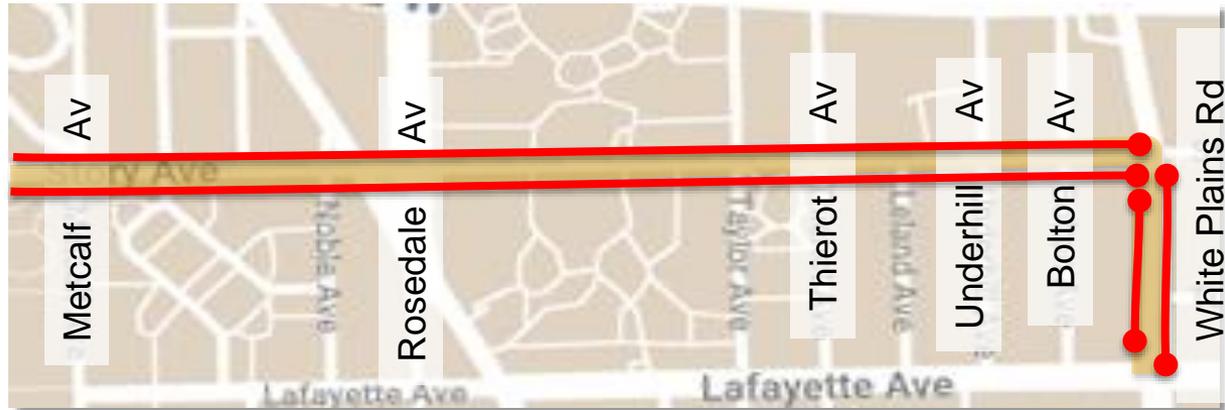
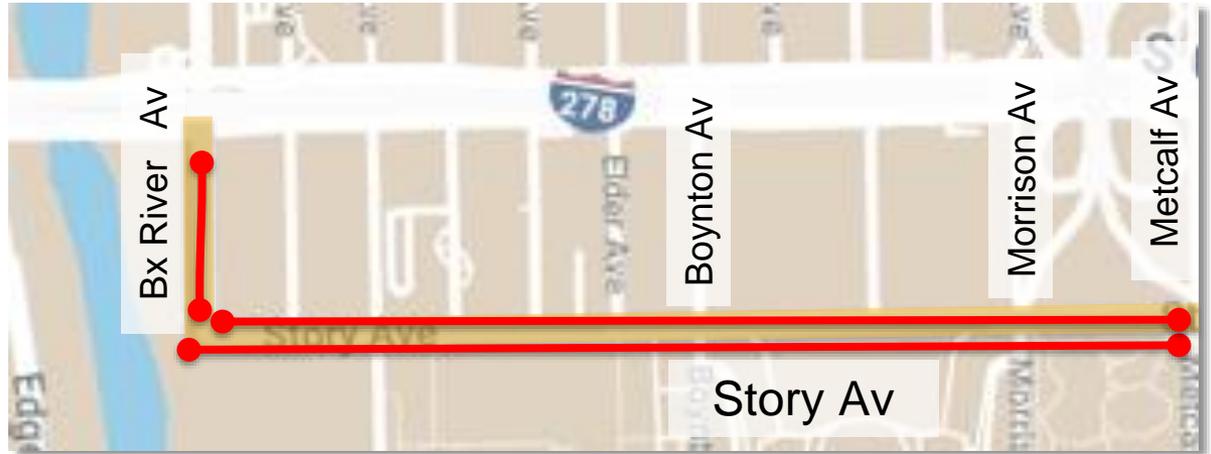
- 16 people killed or severely injured on Story Av (2014 – 2018)
- Story Av intersects with Vision Zero Priority Corridors and Intersections
- In addition to transit improvements, DOT will continue to study safety enhancements for all roadway users

	Total Injuries	Severe Injuries	Fatalities	KSI
Pedestrian	64	2	3	5
Bicyclists	16	2	0	2
Motor Vehicle Occupant	333	9	0	9
Total	413	13	3	16

# Proposals

# Proposed Bus Lanes

- Bus lanes on Story Avenue and White Plains Rd would maintain overnight parking throughout the corridor
- Bus lane on Bronx River Avenue would be in effect at all times

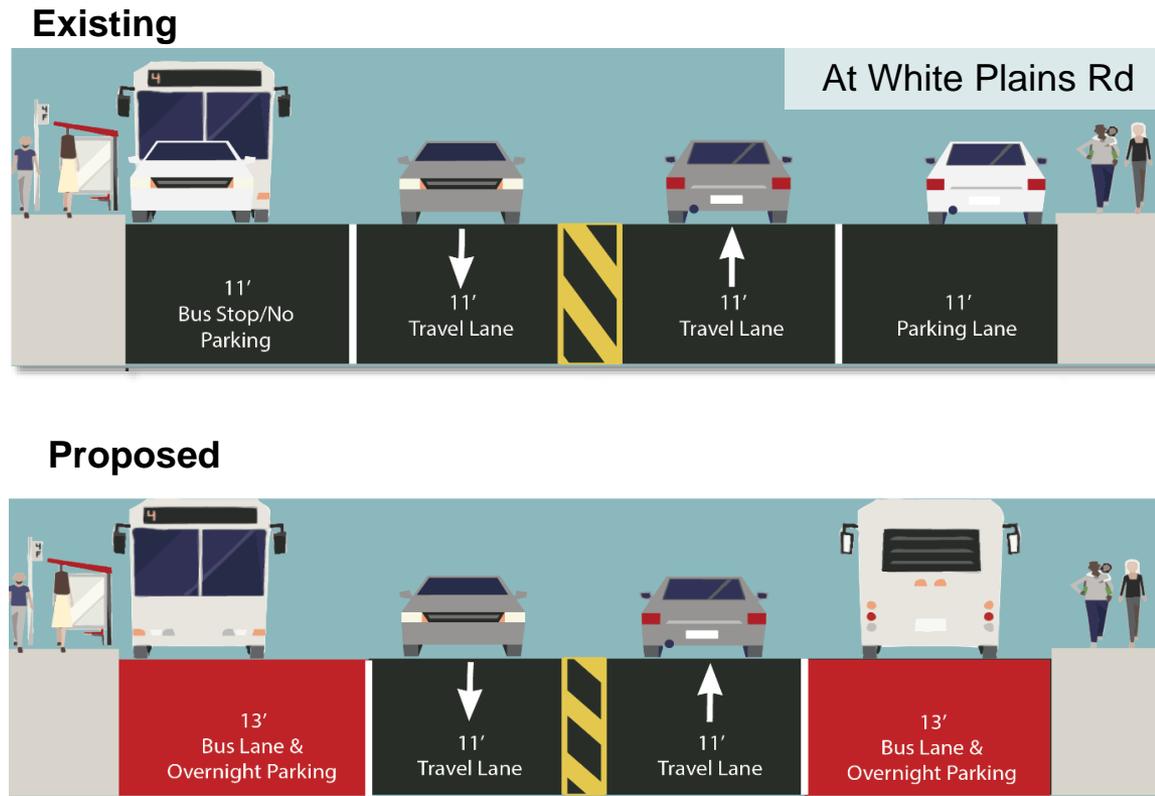


 Proposed offset or curbside bus lane

# Proposed Treatments: Story Av

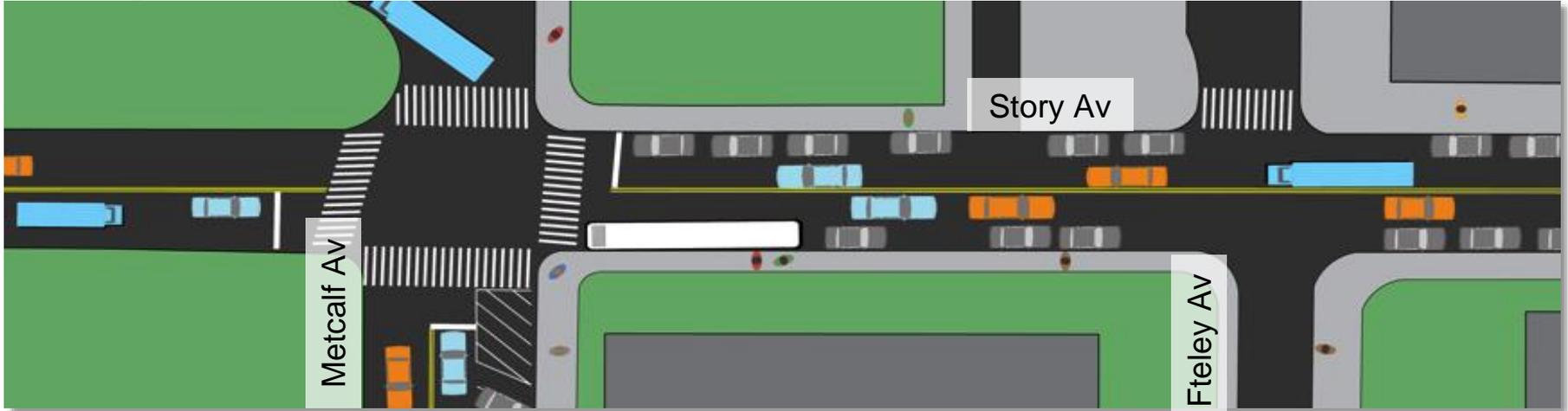
## 1. Curbside Bus Lanes on Story Av between Bronx River Av and White Plains Rd

- Issues identified:
  - Slow and unreliable bus service along congested corridor
- Curbside Bus Lane Benefits:
  - Provides dedicated road space for buses, increasing speed and reliability
  - Removes buses from general travel lanes, reducing congestion
  - Provides clearance for bus stops
  - Maintains overnight parking

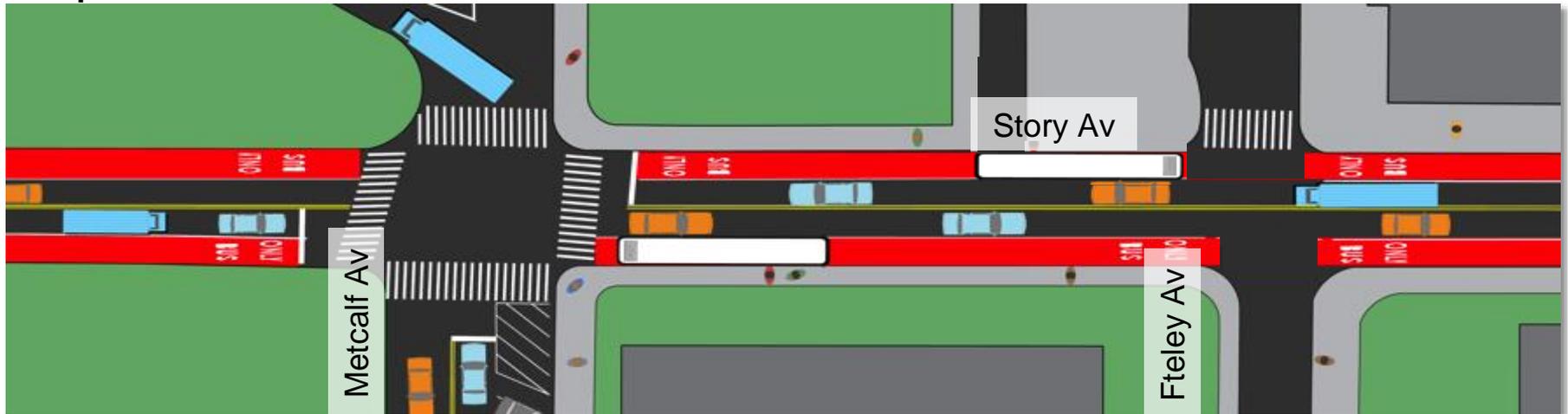


# Proposed: Story Av

Existing



Proposed

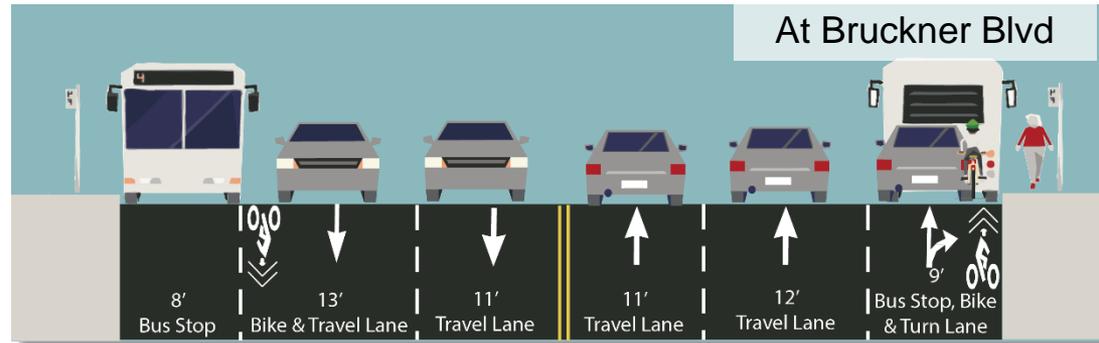


# Proposed Treatments: Bronx River Av

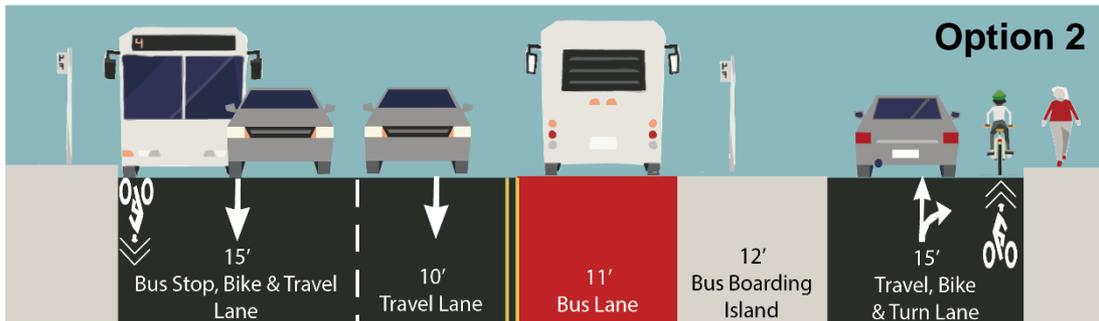
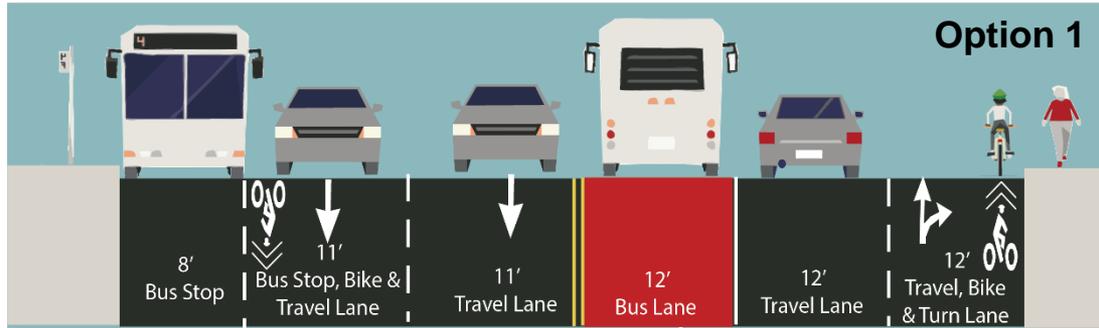
## 2. Northbound Left Side Bus Lane – Being Studied

- Issues identified:
  - Slow and unreliable bus service along congested corridor
- Left Side Bus Lane Benefits:
  - Provides dedicated road space for buses, increasing speed and reliability
  - Aligns bus with left turn at Bruckner Blvd
  - Maintains parking spaces and allows for access for industrial activities

Existing



Proposed



# Proposed Treatments: Story Av between Fteley Av and Croes Av

## 3. Updated Curb Regulations on Story Av between Fteley Av and Croes Av

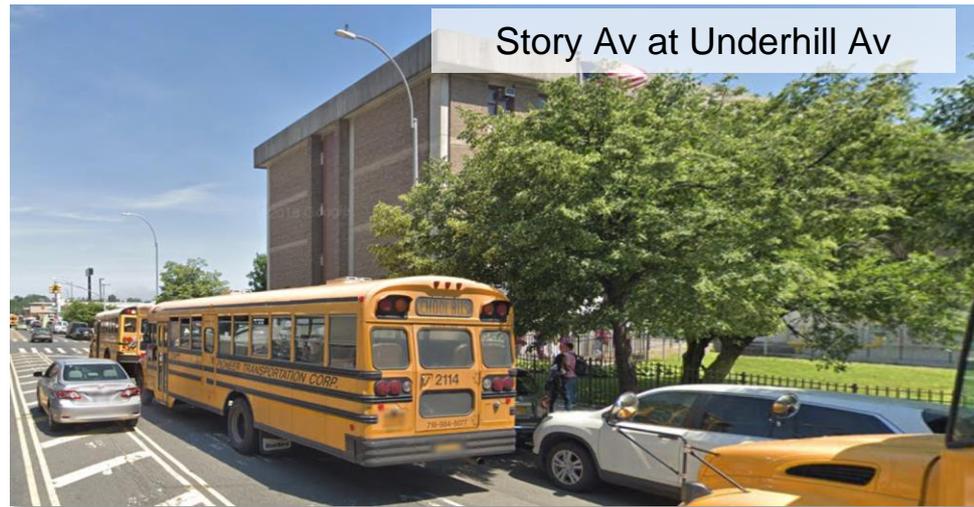


- Issues identified:
  - Due to overflow parking for NYPD vehicles and no parking at FDNY fire zone, vehicles currently block bus stops or limit bus access to curb
  - Creates traffic, especially for buses and cars travelling eastbound and westbound in peak AM/PM hours
- Updating Curb Regulations Benefits:
  - Improves demand management, formalizes parking for authorized vehicles in designated parking areas
  - Facilitates movement of all other vehicular traffic traveling eastbound and westbound
  - Emergency vehicles can travel in bus lanes at all hours

# Proposed Treatments: Story Av at Underhill Av

## 4. Updated Curb Regulations on Story Av at Underhill Av

- Issues identified:
  - Due to Alternate Side Parking (currently 7AM to 4PM on school days), school buses must double park for student pick-up on school days during peak hours
  - Creates traffic, especially for buses and cars travelling eastbound in peak AM/PM hours
- Updating Curb Regulations Benefits:
  - Bus lane for helps with demand management, prioritizing school buses during student pick-ups and drop-offs
  - Facilitates movement of all other vehicular traffic travelling eastbound



# Proposed Treatments: Mid-block Crossings

## 5. Mid-block Pedestrian Crossing – Being Studied

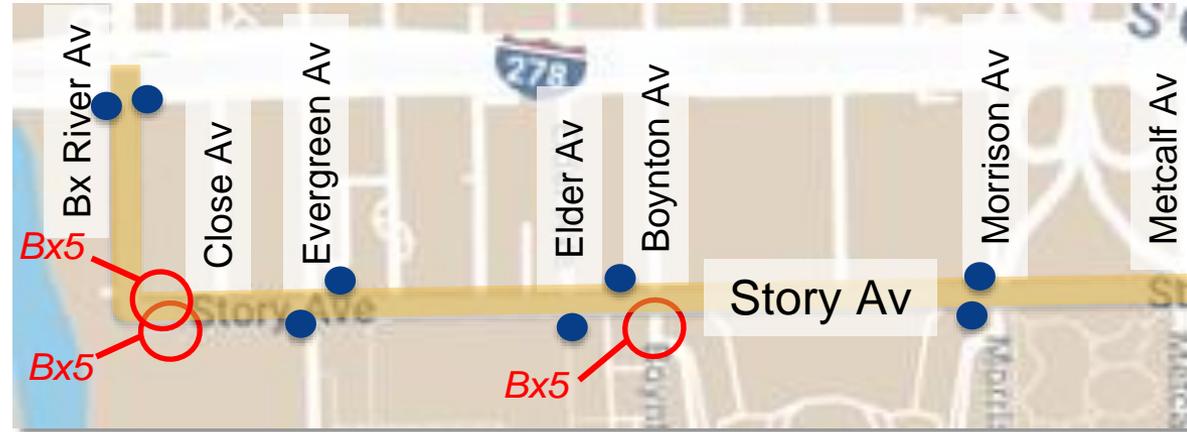
- Issues identified:
  - Story Av between Rosedale Av and Taylor Av is a long block without dedicated pedestrian crossing
- Benefits of treatment:
  - Improve pedestrian safety for those accessing bus stops and crossing Story Av
- DOT is currently studying the feasibility of new, mid-block pedestrian crossing



# Proposed Treatments: Bus Stop Changes

## 6. Consolidated Bus Stops

- To improve bus speeds and reliability, seven bus stops will be consolidated on the corridor. These changes allow for additional overnight parking spaces



● Bx5 Local bus stop

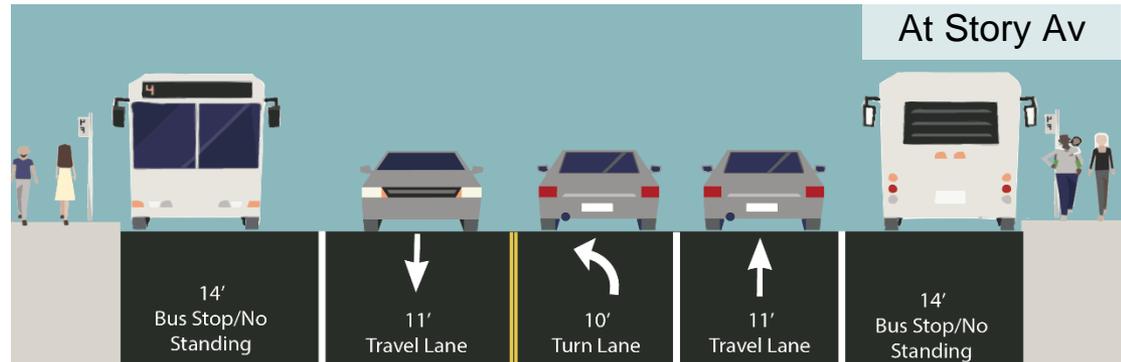
○ Bx5 Local bus stop to be removed

# Proposed Treatments: White Plains Rd

## 7. Curbside Bus Lanes on White Plains Rd between Story Av and Lafayette Av

- Issues identified:
  - Slow and unreliable bus service along congested corridor during peak hours
- Curbside Bus Lane Benefits:
  - Provides dedicated road space for buses, increasing speed and reliability
  - Removes buses from general travel lanes, reducing congestion
  - Provides clearance for bus stops

Existing



Proposed



# Parking Changes on Story Av

- Parking would be preserved overnight
- Stop consolidations would add up to 16 parking spaces overnight

## Story Av, Bronx River Av to White Plains Rd (EB & WB)

	Parking	
	South Curb	North Curb
<b>Current</b>	191	146
<b>Proposed</b>	Segments of curbside bus lanes in effect during peak hours.	
<b>Full-time Parking Changes</b>	+11	+5
	Full-time parking spaces gained from bus stop removals	
<b>Part-time Parking Changes</b>	-172	-130
	Alternate side parking spaces repurposed during daytime bus lane hours; preserved for overnight parking	

# Proposed Treatments: Story Av

## Summary of Proposed Treatments

1. Curbside Bus Lanes on Story Av between Bronx River Av and White Plains Rd
2. Northbound Left Side Bus Lane on Bronx River Ave – *Being Studied*
3. Updated Curb Regulations on Story Av between Fteley Av and Croes Av
4. Updated Curb Regulations on Story Av at Underhill Av
5. Mid-block Pedestrian Crossing – *Being Studied*
6. Consolidated Bus Stops between Bronx River Av and White Plains Rd
7. Curbside Bus Lanes on White Plains Rd between Story Av and Lafayette Av

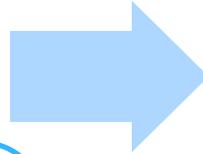


# Project Schedule

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April 2021

- Present draft plan to CB9
- Finalize design based on stakeholder feedback
- Share final plan with CB and other stakeholders



Summer 2021

- Install markings and signage
- Adjust parking regulations
- Coordinate with NYSDOT on Bruckner reconstruction

# Thank You!

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## Questions?



NYC DOT



NYC DOT



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